



TOWNSHIP OF WELLINGTON NORTH

TO: Mayor and Council
DATE: 2024-11-04
MEETING TYPE: Open
SUBMITTED BY: Tammy Stevenson, Senior Project Manager
REPORT #: INF 2024-021
REPORT TITLE: John Street and Fergus Street North Reconstruction Community Survey and Design Update

RECOMMENDED MOTION

THAT the Council of the Corporation of the Township of Wellington North receive Report INF 2024-021 being a report on the John Street and Fergus Street North Reconstruction Community Survey and Design Update;

AND THAT Council direct staff to proceed with John Street as shown in Triton Design Option 1A including:

- Roadway constructed to Municipal Servicing Standards for urban design at 8.5m wide with two through lanes of traffic, one in each direction and maintaining the existing John Street access to the Tim Horton.
- Barrier curb and gutter.
- Sidewalk installed on west side of roadway adjacent to curb at a width of 1.5m.
- Minimal hydro pole conflicts.
- Minimal construction impacts on trees.

PREVIOUS REPORTS/BY-LAWS/RESOLUTIONS

INF 2024-016 Fergus Street Reconstruction Update

INF 2024-015 John Street Reconstruction Update

OPS 2023-043 Public Information Centre – Capital Projects

INF 2024-010 Spring Traffic Count

BACKGROUND

Design updates for John Street and Fergus Street North projects were presented to Council in staff reports at the September 23, 2024, Council Meeting. A Community Survey to solicit feedback on the John Street and Fergus Street North Reconstruction projects was posted to the Township website. Public comment was received between September 25 and October 9,

2024. Promotion of the survey was provided on social media platforms and the Township website.

ANALYSIS

The consolidated survey results have been included in this report, as shown in Attachment 1. In total 124 survey responses were received. The public is generally supportive of any option that limits the impact to tree removal. The public has concerns with the existing traffic issues with Tim Horton's drive through vehicles queuing on John Street and the impact to vehicle movement and access residential properties along John Street.

The main objective of the John Street and Fergus Street North Reconstruction projects is to replace the ageing infrastructure (watermain, sanitary sewers, storm sewers and roadway) that is beyond the anticipated lifecycle and identified in the Asset Management. In order to advance these projects to construction stage, staff are recommending the following:

Fergus Street North Reconstruction Project be constructed as shown in B.M. Ross and Associates Limited Design Option, as shown in Attachment 2.

John Street Reconstruction Project be constructed as shown in Triton Design Option 1A, as shown in Attachment 3, which includes:

- Roadway constructed to Municipal Servicing Standards for urban design at 8.5m wide with two through lanes of traffic, one in each direction and maintaining the existing John Street access to the Tim Horton.
- Barrier curb and gutter.
- Sidewalk installed on west side of roadway adjacent to curb at a width of 1.5m.
- Minimal hydro pole conflicts.
- Minimal construction impacts on trees.

The design team acknowledges that there are ongoing traffic issues on John Street and will be monitoring and communicating with stakeholders and seeking additional bylaw enforcement when required. With the construction of John Street to urban standards including installation of new barrier curb and gutter, queuing traffic to turn left into Tim Hortons may feel uncomfortable waiting in the active lane of traffic. The barrier curb and gutter will also give the appearance of a narrower roadway and may discourage vehicle traffic from illegally stopping on John Street. John Street roadway base will also be constructed to allow for future modification as required.

Alternatively, Council may consider recommending a different design option as follows:

“AND THAT Council direct staff to proceed with John Street Design Option X.”

CONSULTATION

Brooke Lambert, Chief Administrative Officer
Jerry Idialu, Director of Finance/Treasurer
Dale Clark, Manager of Transportation Services
Corey Schmidt, Manager of Environmental Services
Triton Engineering Services Limited

FINANCIAL CONSIDERATIONS

There are no financial considerations to receive this report.

ATTACHMENTS

Attachment 1 – Public Survey results from September 25 to October 9, 2024

Attachment 2 – B.M. Ross and Associates Limited Fergus Street N Design

Attachment 3 – Triton Engineering Services Limited John Street Design Options

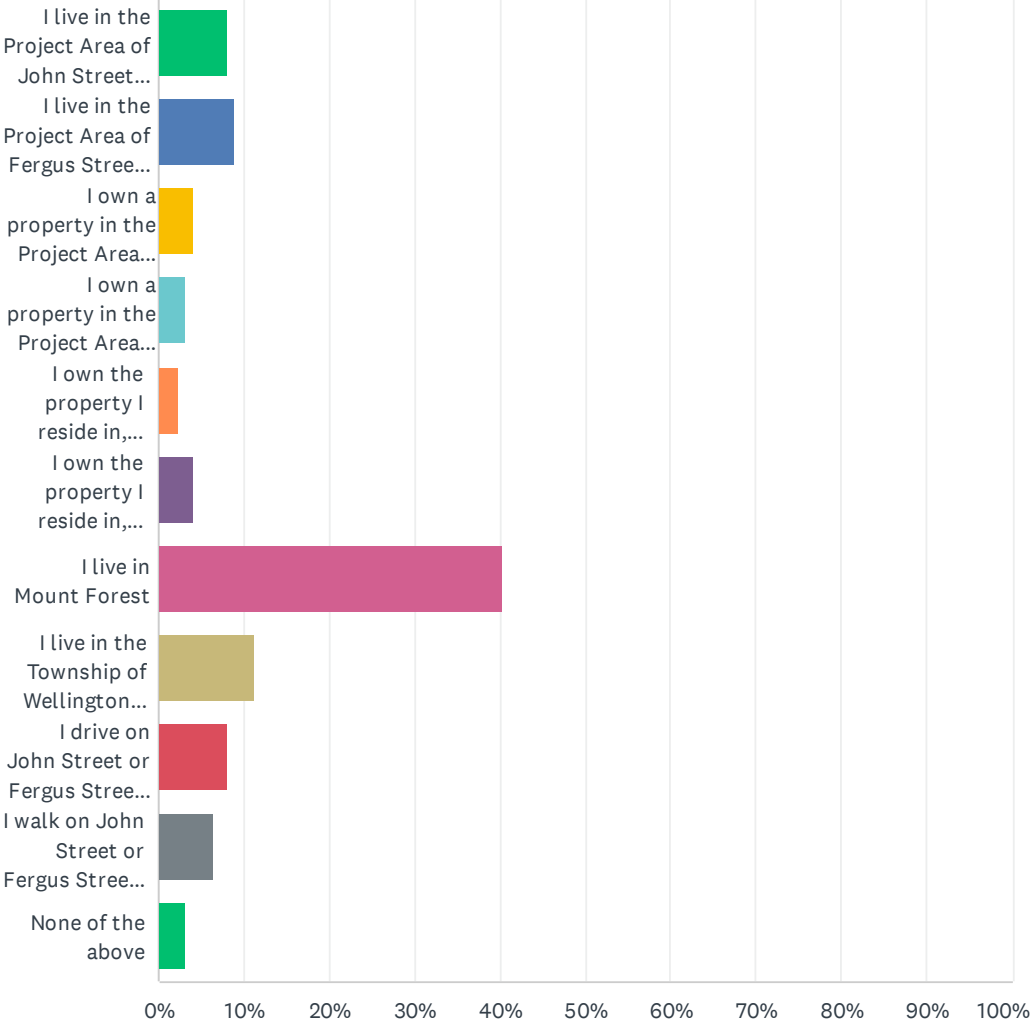
STRATEGIC PLAN 2024

- Shape and support sustainable growth
How:
- Deliver quality, efficient community services aligned with the Township's mandate and capacity
How:
- Enhance information sharing and participation in decision-making
How:
- N/A Core-Service

Approved by: Brooke Lambert, Chief Administrative Officer

Q1 How does this project relate to you? (Pick one)

Answered: 124 Skipped: 0

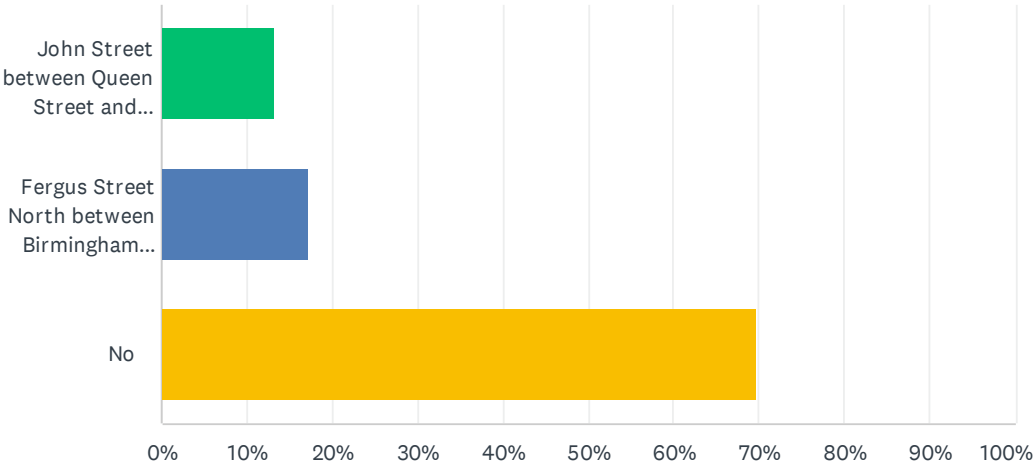


John Street and Fergus Street North Reconstruction Projects Public Survey – September 25 to
October 9, 2024

ANSWER CHOICES	RESPONSES	
I live in the Project Area of John Street between Queen Street West and Waterloo Street	8.06%	10
I live in the Project Area of Fergus Street North between Birmingham Street East and Sligo Road East	8.87%	11
I own a property in the Project Area of John Street between Queen Street West and Waterloo Street	4.03%	5
I own a property in the Project Area of Fergus Street North between Birmingham Street East and Sligo Road East	3.23%	4
I own the property I reside in, which is in the Project Area of John Street between Queen Street West and Waterloo Street	2.42%	3
I own the property I reside in, which is in the Project Area of Fergus Street North between Birmingham Street East and Sligo Road East	4.03%	5
I live in Mount Forest	40.32%	50
I live in the Township of Wellington North	11.29%	14
I drive on John Street or Fergus Street North	8.06%	10
I walk on John Street or Fergus Street North	6.45%	8
None of the above	3.23%	4
TOTAL		124

Q2 Do you live or own property within the project limits?

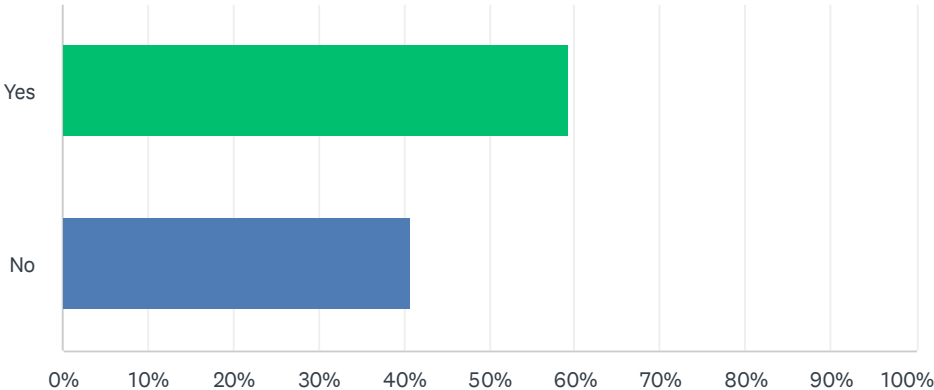
Answered: 122 Skipped: 2



ANSWER CHOICES	RESPONSES	
John Street between Queen Street and Waterloo Street	13.11%	16
Fergus Street North between Birmingham Street and Sligo Road	17.21%	21
No	69.67%	85
TOTAL		122

Q3 Do you have a sump pump?

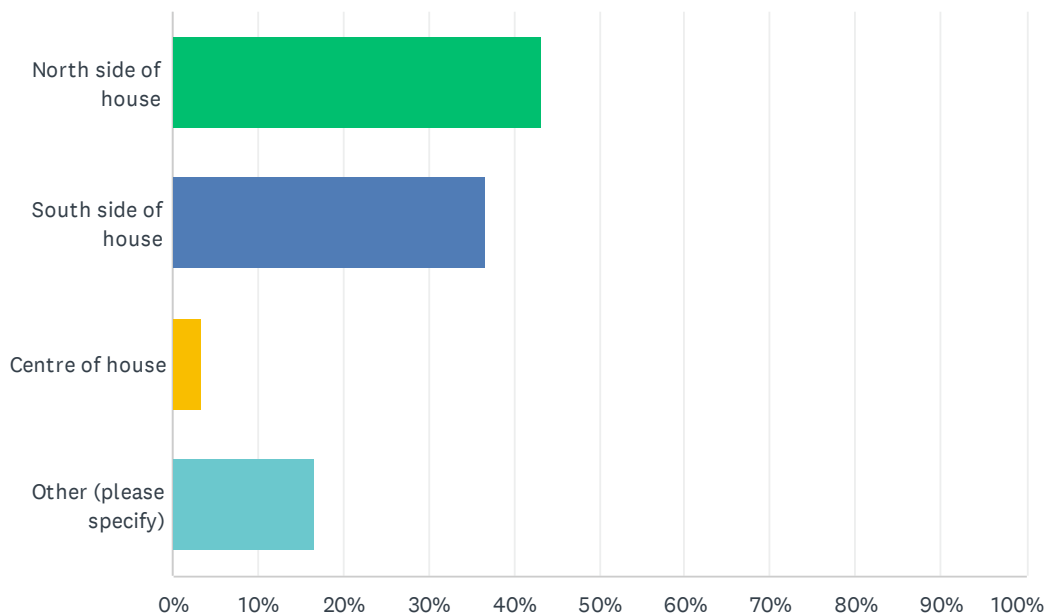
Answered: 32 Skipped: 92



ANSWER CHOICES	RESPONSES	
Yes	59.38%	19
No	40.63%	13
TOTAL		32

Q4 Preferred location for municipal storm service located at property line for an outlet for private sump pump connections. Example - North side of house

Answered: 30 Skipped: 94



ANSWER CHOICES	RESPONSES
North side of house	43.33% 13
South side of house	36.67% 11
Centre of house	3.33% 1
Other (please specify)	16.67% 5
TOTAL	30

#	OTHER (PLEASE SPECIFY)	DATE
1	Not sure	10/9/2024 3:44 PM
2	Does not apply	9/29/2024 8:42 AM
3	I do not need a hook up. It is taken care of.	9/27/2024 2:29 PM
4	I do not need it. All looked after	9/27/2024 2:22 PM
5	I have no sump pump pail in the basement	9/26/2024 9:21 PM

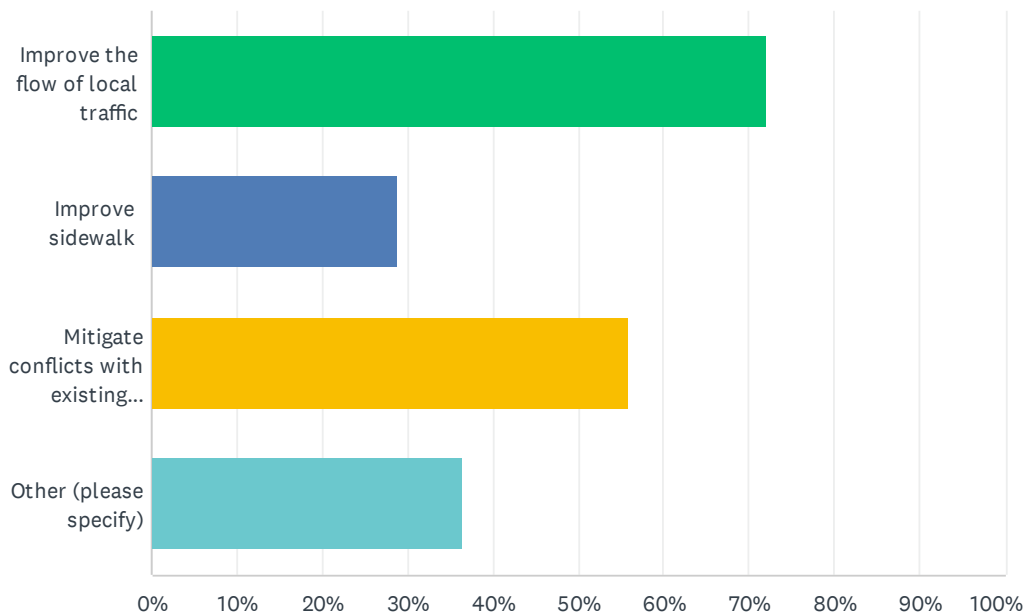
Q5 Do you have any comments specific to your property?

Answered: 11 Skipped: 113

#	RESPONSES	DATE
1	What is happening to my property?	10/9/2024 3:44 PM
2	I am thankful that some of the trees are not being removed now. However, there is still one tree on my property that is to be removed. It is a mature and very healthy tree. Can you please explain to me the reason for the removal?	10/8/2024 8:57 AM
3	No	9/30/2024 6:52 PM
4	stop the traffic on john street and do not cut down trees on a residential street for a commercial business if they cannot accommodate their own traffic they need to move	9/29/2024 9:54 PM
5	Put a new sidewalk on the West side of the street. Creates a safer situation for sidewalk users. Same side of street Sligo to 89. No crossing over.	9/29/2024 8:42 AM
6	Try to save our tree at the front of the house	9/27/2024 2:29 PM
7	None	9/27/2024 9:34 AM
8	Will my new driveway be torn up and replaced ?	9/27/2024 7:56 AM
9	No	9/26/2024 9:21 PM
10	Don't touch my trees	9/26/2024 4:40 PM
11	i hope i will not lose any trees	9/26/2024 3:41 PM

Q6 What aspects of the John Street reconstruction project are most important to you? Select two (2).

Answered: 104 Skipped: 20



ANSWER CHOICES	RESPONSES	
Improve the flow of local traffic	72.12%	75
Improve sidewalk	28.85%	30
Mitigate conflicts with existing municipal trees	55.77%	58
Other (please specify)	36.54%	38
Total Respondents: 104		

#	OTHER (PLEASE SPECIFY)	DATE
1	Trees should not come down.	10/9/2024 3:45 PM
2	Not interested	10/8/2024 4:34 PM
3	minimize disruption to surrounding properties	10/8/2024 8:57 AM
4	Stop people from blocking the street with distracted drivers	10/5/2024 2:26 PM
5	Remove unlawful commercial traffic from residential road and enforce the law with physical measure built from concrete as signage and by-laws have failed for decades to deliver. Restore a balance between all stakeholders.	10/3/2024 2:10 PM
6	Stop the abuse of the existing properties by Tim customers waiting to access the drive through. Some people acknowledge the existing driveways and do not block them but others are oblivious and do what they please.	10/1/2024 1:24 PM
7	Find a solution to the Tum Hortons drive through	10/1/2024 5:54 AM
8	I want to ensure that the best option includes either no removal of trees or a minimum removal	9/30/2024 8:36 AM

John Street and Fergus Street North Reconstruction Projects Public Survey – September 25 to October 9, 2024

of trees. I also believe it is very important to not impact property owners directly affected.

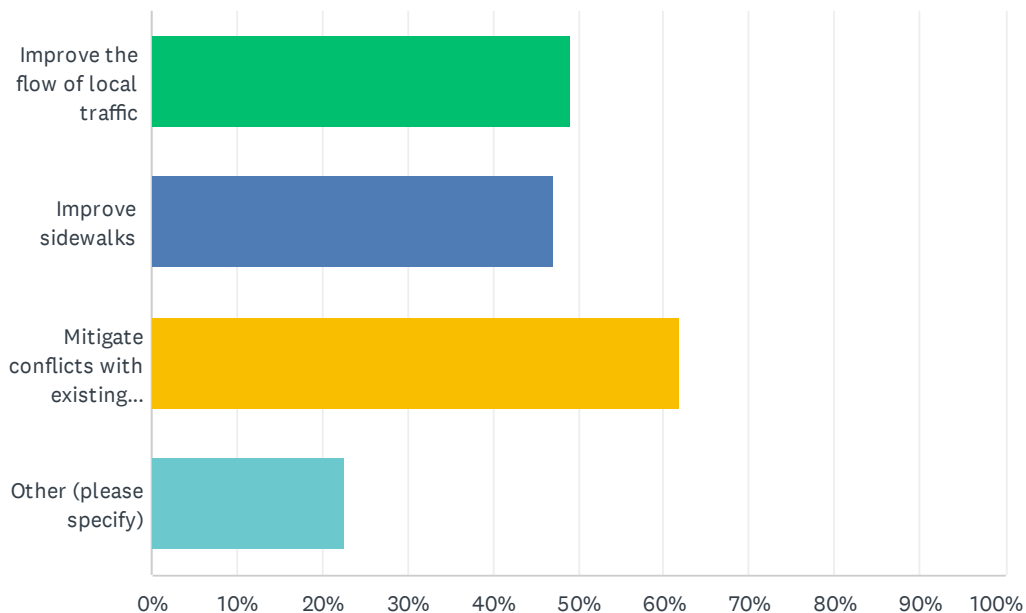
9	My husband experienced a fender bender there and I am also concerned about pedestrians and local hone owners	9/30/2024 7:58 AM
10	Traffic heading west on Queen (89 highway) is constantly backed up with traffic turning left on to John street. Interfering with the flow	9/29/2024 6:21 PM
11	Keep trees	9/28/2024 4:47 PM
12	What is the cost of the project	9/28/2024 1:40 AM
13	Save the trees	9/27/2024 8:20 PM
14	Get new pipes	9/27/2024 2:24 PM
15	My preferred design is Option #2 as it would eliminate the queuing of traffic altogether in the no stopping zones. Option 3 is also a good idea but from what I've seen in other locations in Guelph and Fergus, there will sill be southbound drivers trying to enter the northbound Tim Hortons exit. Option 5 will still have drivers stacking up on the northbound side in the No Stopping zone.	9/27/2024 9:52 AM
16	Tim hortons	9/27/2024 8:58 AM
17	Don't care if Tim hortons closes or moves. I don't go there. The citizens on John Street are being inconvenienced and historic trees removed to accommodate traffic flow for coffee	9/27/2024 7:59 AM
18	Municipally-owned services get fully replaced as part of the project	9/27/2024 5:22 AM
19	Not use taxpayers' money to resolve a traffic flow issue that a private business needs to take responsibility for. Close John St entry access to minimize impact to town residents.	9/27/2024 12:59 AM
20	that all voices and ideas are truly considered	9/26/2024 10:35 PM
21	Resurface of the street asphalt	9/26/2024 9:22 PM
22	Tim hortons drive through	9/26/2024 8:56 PM
23	Allow the trees to be left alone	9/26/2024 8:40 PM
24	Eliminate Tim Horton s drive through traffic on John street	9/26/2024 8:11 PM
25	Stop congestion on 89. People can not move east to west due to people wsiating to turn onyo John St	9/26/2024 7:48 PM
26	leave it as is , the problem is sporadic and large percentage of the day and days of the week it is not a problem . Has anyone contacted the owners of Tim's and see if they can limit the use of the drive thru to product for the occupants only , A single occupant purchasing 8 coffees and food products should be differed to in store counter service, then the line would proceed faster. When you take into account that the Funeral home, Petro can and corner gas and convenience and Tims have 11 entry and exit points at 89and 6 Hwy, we do not want to transfer the congestion to the hwys. Not a problem and my tax money could be spent more productively.	9/26/2024 7:06 PM
27	Stop the tim Hortons parking lot on John st	9/26/2024 6:52 PM
28	The drive thru lane from John Street and exit onto John Street associated with Tim Horton business causes congestion on all surrounding traffic flow. Closing the John St access completely makes most sense. The business has altered their property from the original plan by adding curbs, large stones and a green space. That access was only for service vehicles in 1995. The Horton supply Truck parks on John Street, in no parking, to deliver product by wheeled dolly	9/26/2024 6:49 PM
29	I think this project is unnecessary! Absolutely do not want to lose any large trees! Tim Hortons should deal with their drive through themselves	9/26/2024 6:33 PM
30	Traffic flow but a turning lane isn't going to solve this. There are too many cars. Paint lines to remind people it's a road not a driveway. Bylaw officers handing out fines would work wonders.	9/26/2024 6:24 PM
31	Closing of the drive through at Tim hortons	9/26/2024 6:21 PM
32	Traffic flow	9/26/2024 4:51 PM

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33	Close the back entrance to tim hortons it's there problem	9/26/2024 4:42 PM
34	CLOSE THE DRIVE THRU ENTRANCE, PROBLEM SOLVED!	9/26/2024 4:27 PM
35	Tax dollars should not go towards a turning lane for one business.	9/26/2024 4:17 PM
36	Also to make sure our tax dollars are not enhancing one business. If they want improvements, they can pay for it.	9/26/2024 4:13 PM
37	what the heck why won't it let me go to the next question?	9/26/2024 3:53 PM
38	Save trees from being removed for environmental reasons	9/26/2024 3:38 PM

Q7 What aspects of the Fergus Street North reconstruction project are most important to you? Pick two (2).

Answered: 102 Skipped: 22



ANSWER CHOICES	RESPONSES
Improve the flow of local traffic	49.02% 50
Improve sidewalks	47.06% 48
Mitigate conflicts with existing municipal trees	61.76% 63
Other (please specify)	22.55% 23
Total Respondents: 102	

#	OTHER (PLEASE SPECIFY)	DATE
1	No comment	10/9/2024 3:45 PM
2	Na	10/8/2024 4:34 PM
3	None	10/3/2024 6:24 PM
4	Stop the crazy jam up of impatient people. This is not all local traffic by any stretch. Option 3 not allowing any entrance sounds good. A median would only let people line the street from the south.	10/2/2024 9:12 PM
5	Same response as Question 3	9/30/2024 8:36 AM
6	My husband experienced a fender bender there and pedestrian safety as well as local hone owners	9/30/2024 7:59 AM
7	Maintain trees on our property.	9/29/2024 9:54 PM
8	Save the trees, traffic is fine and so are side walks	9/27/2024 8:20 PM
9	Also we will get new services!!	9/27/2024 2:30 PM

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10	Get new pipes	9/27/2024 2:24 PM
11	Fully replaced municipal services	9/27/2024 5:23 AM
12	Not use taxpayers' money to address a traffic flow problem that should be a private business's responsibility. Close John St. access to Tim's to minimize impact to town residents.	9/27/2024 1:02 AM
13	To see all the large century trees saved!	9/26/2024 8:17 PM
14	Make Tim's pay for it	9/26/2024 7:18 PM
15	As previous	9/26/2024 6:50 PM
16	There is nothing wrong with our sidewalks or traffic flow - seems like a make work project. Absolutely do not take down ANY large trees !!	9/26/2024 6:34 PM
17	Turning lane won't solve this. Paint lines on road to remind people it isn't a driveway. Bylaw officers handing out tickets would be great	9/26/2024 6:26 PM
18	The trees are to large. They need a tree maintenance program	9/26/2024 6:23 PM
19	Traffic congestion	9/26/2024 4:52 PM
20	Trim the trees in town on a regular basis, problem solved!	9/26/2024 4:28 PM
21	When are we going to learn to trim trees? Those old huge maples in town are a danger to society. They need properly maintained, drive down any street, the time is now to start trimming trees to avoid them growing out of control.	9/26/2024 4:20 PM
22	I do not know about the Fergus St N project	9/26/2024 4:02 PM
23	Save trees	9/26/2024 3:39 PM

Q8 Additional comments you would like to make about these projects.

Answered: 50 Skipped: 74

#	RESPONSES	DATE
1	Not sure why taxpayers are paying for Tim Hortons business.	10/9/2024 3:46 PM
2	I sincerely appreciate that a revised plan is being implemented. Thank you.	10/8/2024 8:58 AM
3	John Street is a nightmare. We have contacted OPP who don't care to enforce posted restrictions on stopping and parking. Traffic flow that near two major highway intersecting should be high priority. No turning lanes or anything, but the new grocery store has these in place.	10/5/2024 2:28 PM
4	It seems like the property owners are being penalized and losing space for the Tim horton's traffic. Taking from us to accomodate them.	10/5/2024 8:43 AM
5	I think Tim Horton's holds some responsibility for the traffic issues that their drive through is causing.	10/4/2024 12:34 PM
6	We understod this survey might provide some revised examples on the reconstruction of John Street based on the last council meeting. Dissappointed none were provided. We are very much in favour of either making John a one-way street going North from Waterloo to Queen or as council had discussed make the Tim Hortons access to John Street as only an exit heading north.	10/4/2024 9:37 AM
7	John street construction should be paid for in full by Tim Hortons or they should lose the John street access fully. They designed their mess, tax payers should not bail them out.	10/3/2024 6:27 PM
8	My property is directly impacted by your suggestions. I understand that the township needs to find a solution which creates the most benefit for the community at the lowest cost possible. First, I would like to thank your team for listening to the residents, going back to the drawing board, evaluating multiple options with real traffic data, and pulling the site plan. Currently, Tim Hortons is intentionally using a residential road for their commercial traffic deliberately contrary to the submitted site plan from 1995. This non-compliance has been endured by the neighborhood since then and I doubt changing any traffic rules or adding signs will have an impact considering the current traffic rules which were added in response to Tim Hortons non-compliance are not enforced nor obeyed in this area. The only working traffic rule enforcement in this township is called concrete barriers. Comment for option 1A: This option would create significant traffic chaos and will only make the situation worse, basically handing over the stretch of street to Tim Hortons. Signs did not make people uncomfortable lining up, adding a curb will not change that. It will disable the possibility to pass the line up. This makes my property even worse accessible as it already is. I find this option unacceptable. Comment for option 1B: This option is worse than 1A as it will hand over two lanes to Tim Hortons line up traffic and lock up the neighborhood. Traffic will line up on the east side of the road (no matter the direction of the one way road). Then when the lane is full, the second lane will be used. And when the impatient customer (1 in 10) will arrive honking they will also pretend the first lane is full and use the second one until one lets them in or they are able to force themselves into a gap because one was too long on their phone. It will disable access to my property for hours. I find this option totally unacceptable. Comment for option 4: A center median in front of my driveway is unacceptable. Traffic will utilize the extra space and we can rename the road Tim Hortons drive through. People will line up on the East lane of John Street from Waterloo Street. People arriving from Queen Street to John Street will drive until either the concrete barrier is passed and make a u-turn or just drive on the left side of the road or parking lot when traffic is low. I have seen countless cars shooting from Queen Street into John Street to Tim Hortons at a high speed and driving on the derelict gas station parking lots to bypass John Street lineups or the traffic light. I find this option absolutely unacceptable. Comment for option 5: A dead end street to support even more Tim Hortons drive through traffic and parking in addition to creating a mini park for our visitors to use as a garbage dump and public toilet is just ridiculous. We can rename John Street to Tim Hortons-drive-through-and-parking-road. I find this option absolutely unacceptable. Comment for option 3: This option is at least starting to correctly accept that we are having traffic problems. There are multiple problems solely	10/3/2024 2:11 PM

created by Tim Hortons when evaluating the traffic data you collected and making in-person observations. This approach tries to solve the drive through stacking problem with a sign. Signs haven't worked since Tim Hortons opened. The proposed physical layout of the John Street access will allow customers to line up on John Street and enter through the entrance by simply ignoring that newly added sign. I have seen multiple Tim Hortons locations with this configuration and it does not take long to see people ignoring the do-not-enter sign. The township's non-existent traffic rules enforcement in this area is one of the reasons why I doubt that this sign will solve the problem. The non-collaboration from profit maximizing Tim Hortons indicates that they are OK with customers breaking the law as they have been for decades. The proposed layout will only limit how many cars can enter and exit the John Street access at a time. Nothing else would change. In addition to this, other traffic problems will be retained. The official exit will allow customers to continue evading the traffic light at all costs. They will line up on John Street to turn left and lock up traffic until they can squeeze out, often ignoring oncoming right-ahead traffic. I have seen countless close call accidents by people turning left. Others, who turn right, will have to compete with one of those few opportunities as there is limited space too, again causing a line up on John Street. This is another indicator that John Street/Queen Street West intersection is designed only for a small amount of local residential traffic and is unable to accommodate the ten fold increase by Tim Hortons commercial traffic no matter how we change the road. If we go with Option 3 as you suggested, then we must also reconsider the road profile as a residential road profile is unable to handle the load created by the heavy truck load traffic generated by Tim Hortons. This comes with an increase of costs to the taxpayer I don't agree with. I find this option unacceptable as I doubt the current traffic flow will change. Comment for option 2: This option is the best option I have seen and I find it acceptable. I think removing access to Tim Hortons is the only feasible option. I want this reconstruction project to beautify this neighborhood. Removing the never municipally planned to be hosted Tim Hortons traffic from this road would allow it to stay beautiful for many more years. A site visit will confirm to you that the line up traffic is a significant load for the road and residential roads are not designed for this kind of traffic Tim Hortons generates. Main street in comparison is designed for heavy traffic, especially those heavy trucks I see all the time sinking deeper into the potholes into our current John Street. If we do not block traffic to Tim Hortons then we need to reconsider the street type we plan to build. I want a residential street for residents, a commercial heavy truck load street should be only installed on a main street but it would be needed for this replacement if we do not want to rebuild the road in 5 years again. Something I would prefer not to do. I understand the heavy traffic road would be more expensive and I don't think the township should use tax funds to accommodate Tim Hortons profit maximizing wishlist. Option 2 allows for most trees to survive and restores the residential character to this neighborhood. Commercial traffic should have never been allowed to take over this road and the submitted site plan should have been enforced a long time ago. As Tim Hortons obviously sees profit as more important than following the law or site plan we have no choice but to physically remove access for the public to Tim Hortons from John Street. Any proposed traffic signage will have the same effect as the current one, none. The only signs obeyed in this town are mounted on concrete barriers. As a directly impacted property owner who has endured this lawless traffic situation with countless close call accidents and being prevented from entering or leaving my property by Tim Hortons customers lining up in front of my driveway, I want option 2 to be implemented. The residential road is for residents and not for commercial use. That is what Main Street is designed and built for. The current plan (option 3) to rebuild John Street as a residential road will not be able to handle the 10 fold increase of traffic created by Tim Hortons and will crumble under our feet within years instead of decades. In summary option 2 is the only option which will address the traffic problems you have identified. All other options will make it worse. I do not understand why you would propose option 3 which basically allows for the current Tim Hortons traffic to continue as it is with a narrowing of the Tim Hortons John Street access and adding another sign. The currently installed signs are not enforced and obeyed and I have seen many other Tim Hortons locations where those do-not-enter signs are ignored constantly, even in jurisdictions with traffic law enforcement. The only traffic rules enforced in this area are made out of concrete. It is the duty of the township to build our roads for dedicated lawful traffic and physically limit unlawful activities when signage fails to deliver. During the day of the traffic study you closed the Tim Hortons access to John Street, I have observed the most peaceful day in this neighborhood. The lineup at Tim Hortons was organized and the number of cars honking and swearing was significantly down. Originally, I assumed customers were just avoiding the location but your traffic data shows that was not the case. "Somehow", having a proper lineup instead of 3 different ones merging is better for everyone, including Tim Hortons (better line up = more cars = more profit). And the cars driving on Queen Street did not have to stop for cars waiting to get on John Street's illegal drive through extension. It was such a successful

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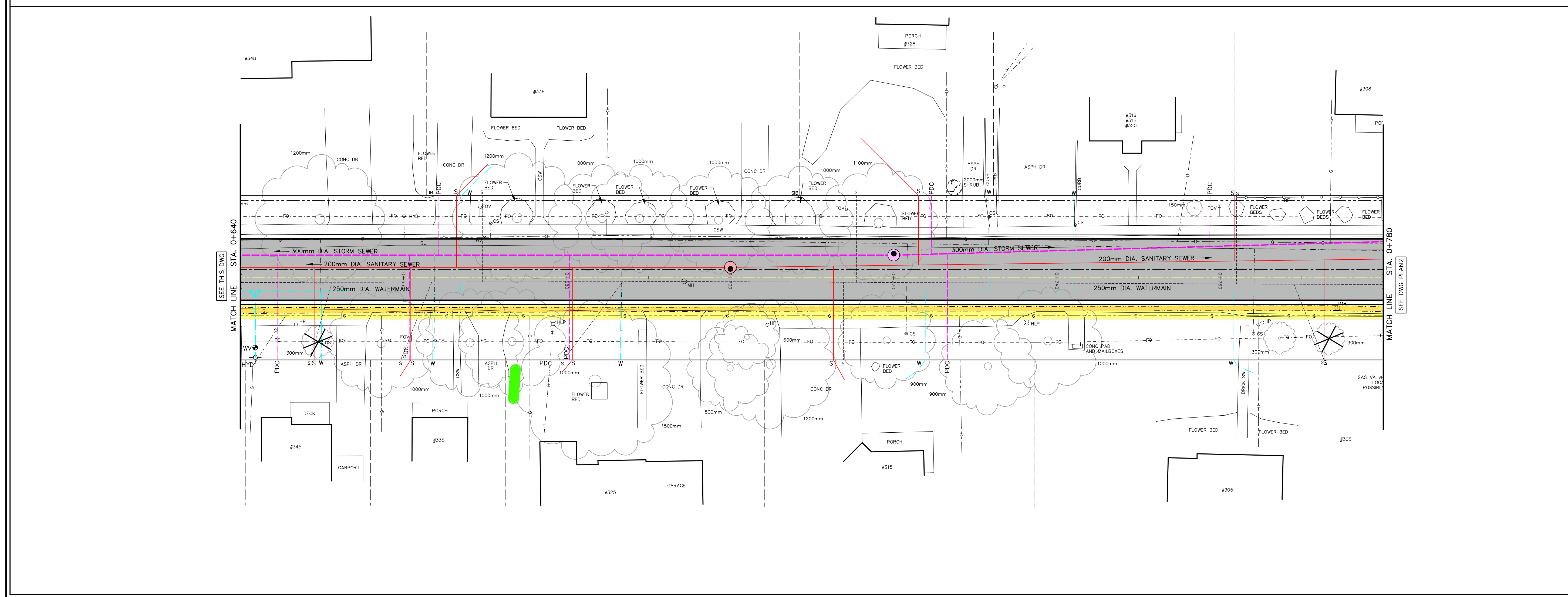
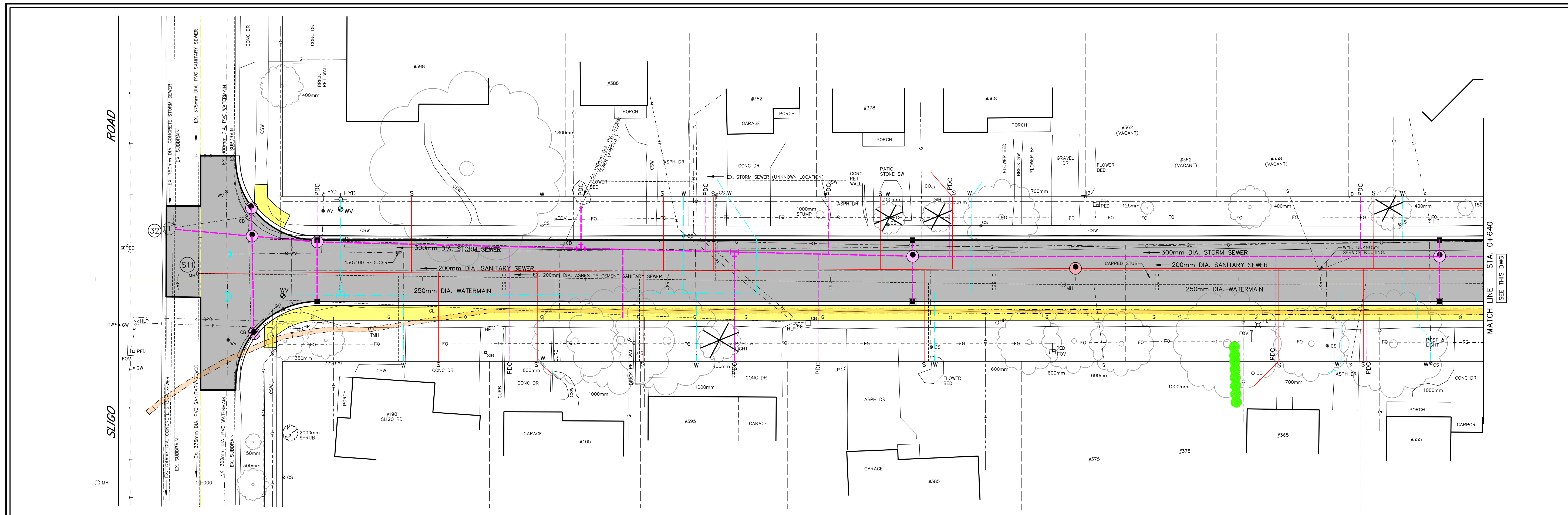
experiment, we should implement it permanently. Option 2 is the only logical choice we have, allowing us to restore the residential character of this road and neighborhood while accounting for all individuals rights. Tim Hortons has seized many of those rights exclusively for way too long and the township has the duty to restore a balance. As by-laws and signage are not working it has no choice but to make a physical change impeding continued deliberate corporate profit centered non-compliance.

9	Good luck. Doing something is better than nothing	10/2/2024 9:13 PM
10	Close John st entrance to tims. That business has altered their site and could remove grass area and rocks to have more drive there space. Opening onto John in 1995 plan was for service vehicles only, not car traffic	10/2/2024 11:20 AM
11	People should not lose a portion of their property and their tree coverage to accommodate some ignorant drivers.	10/1/2024 1:25 PM
12	Taxpayers should not be beholden to the whims of a billion dollar company and their revenues	10/1/2024 11:15 AM
13	It is extremely unfortunate that the Township cannot use the lot that has been vacant for many years on the corner of Highway 6 and 89. It is my understanding that it is privately owned and would need mitigation of the soil, however has this been considered through appropriation of this parcel of land?	9/30/2024 8:40 AM
14	The gentlemen who offered insite with army or navy trafficking flow experience would be a great asset in this project. Also no other business in town is offer3d the kind of support Tim hortons has in this situation. Dont get me wrong I love my tims. But it's glaringly obvious preferential	9/30/2024 8:01 AM
15	keep tim horton's traffic in their parking lot.	9/29/2024 9:55 PM
16	People and their safety come first. This project needs to put safety first, money second but please plant more trees to replace whatever needs to be removed.	9/29/2024 7:34 PM
17	The sidewalk was taken away from the east side yet there is an apartment building and church	9/29/2024 6:24 PM
18	Hire an engineer who knows what they are doing. Sewer pipes should be in straight lines. Not curved around trees. Unbelievable!	9/29/2024 8:47 AM
19	I thought WN was seeking feedback on the John Street options.	9/27/2024 4:30 PM
20	Would like a time line	9/27/2024 2:31 PM
21	Saving the trees in Mount Forest is very important. Also the traffic congestion from Tim Horton's is not a town problem it is a business issue and cost and action should be on that business. Having the town taxes pay for business issues is a dangerous president to set. Having exit north bound only from Tim's onto John Street is the best solution as presented . Residential streets should be kept residential for those who live there and enjoy their homes.	9/27/2024 10:18 AM
22	I see option #2 as the only solution for eliminating the traffic congestion problem	9/27/2024 9:55 AM
23	Removing trees will deflate current values of homes on this street.	9/27/2024 8:01 AM
24	Do not leave aged infrastructure in service in order to save a tree; reconstruction happens infrequently and risk associated with leaving in service aged infrastructure is costly long-term	9/27/2024 5:25 AM
25	We need to leave the trees alone...I was recently in Niagara on the Lake and they have no sidewalks which I am sure was done to leave all the trees intact	9/26/2024 8:42 PM
26	ONE WAY,EITHER WAY YOU WISH,DON'T DESTERB THE TREES.THEY ARE INNOCENT.	9/26/2024 8:22 PM
27	Municipality should not be financially responsible for traffic management for Tim Hortons drive through. Simply close entry access from John street and let Tim hortons figure out the solution.	9/26/2024 8:15 PM
28	I always thought it would be a great idea for Tim Hortons to purchase the old gas bar and MAX have it tore down and extend their parking lot there and have the drive-through come up and around kind of how Hanover Tim Hortons is Where are you pull in right and up through the drive-through and then parking in the middle and up at the store	9/26/2024 8:06 PM
29	Traffic flow was improved downtown and at the same time lost prime parking spaces for upto 9 businesses at Main and Wellington. These businesses lost prime parking spots in the name if	9/26/2024 7:53 PM

John Street and Fergus Street North Reconstruction Projects Public Survey – September 25 to October 9, 2024

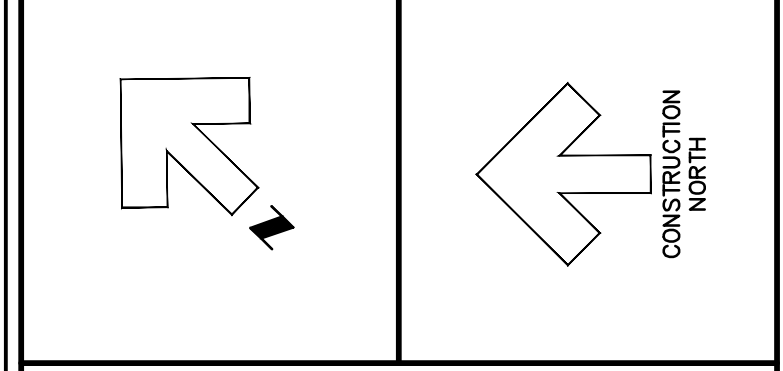
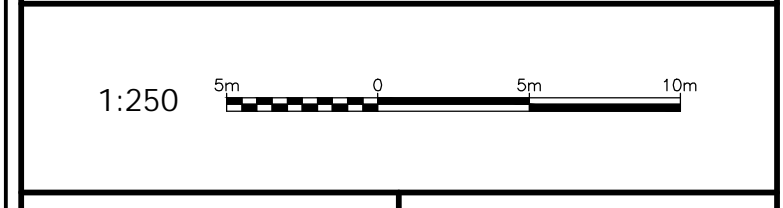
traffic flow. The John Street changes seem to give one business better access - That rubs me the wrong way - and I dont think its right

30	It is a very sad day wgen tax payers need to pay fir a problem that a very profitable corparation has crested in a small town!!!!	9/26/2024 7:49 PM
31	Make Tim's pay for it	9/26/2024 7:18 PM
32	closing John St. to exit only from Tims will force the problem to 6 hwy, Tims also use the John St entre for deliveries, Leave it as is, most of the day like 90% it isn't an issue	9/26/2024 7:10 PM
33	I personally think that closing off the tim hortons drive through is going to cause a variety of accidents.	9/26/2024 6:58 PM
34	Make half of John street a one way street so no need to take down any trees and can help with traffic	9/26/2024 6:31 PM
35	This is a Tim Hortons issue not a tax payer issue. Hand out tickets to people stopped on John St that block driveways or block the road.	9/26/2024 6:29 PM
36	The township plants the trees and then doesn't maintain them. Where is the plan and dollars to do that. Something that has been forgotten for years	9/26/2024 6:24 PM
37	Save as many trees as possible or pant new ones if any need to be removed.	9/26/2024 6:02 PM
38	Make John St. One way from North to south and prohibit any exits from Tim Horton onto John St.	9/26/2024 6:01 PM
39	Reducing sidewalks to just one side of the street increases the number of pedestrians on the roadways, and impacts the safety of both pedestrians and driveways. I live and own property on a street adjacent to Fergus St., and myself and my family frequently drive and walk on Fergus St.	9/26/2024 5:13 PM
40	Good work with the designs!	9/26/2024 5:01 PM
41	This should be decided based on the experts opinions on how best to improve traffic flow while ensuring safety- not based on public opinion.	9/26/2024 4:52 PM
42	It's time the township stands up to ppl and stop allowing the ppl the run the show	9/26/2024 4:43 PM
43	Leave our trees alone	9/26/2024 4:42 PM
44	It's always disheartening to lose any of our beautiful trees. While some may be necessary due to rot and decay we ask you to save all trees if possible.	9/26/2024 4:29 PM
45	Don't delay and just get it done efficiently and quickly	9/26/2024 4:25 PM
46	I think we have learned a lesson from this captial project before it even began. As the Green Committee stated, we need regular tree maintenance to maintain our trees. 100 yr old maples should never grow as tall as they are on Fergus Street. This is so dangerous during storms. Our township has neglected all trees with no maintenance. Lets take care and maintain what we have before they grow out of control.	9/26/2024 4:16 PM
47	None	9/26/2024 4:03 PM
48	municipal residential streets should not be altered at the expense of the tax payers for PRIVATE commercial convenience	9/26/2024 3:45 PM
49	We are in a time that we know that removing trees does not benefit the beauty of health of a community	9/26/2024 3:40 PM
50	This road is a mess. I feel awful for people who live on the street, the Tim Hortons issue is huge. There's someone trying to sell right across the road and I imagine it'll be mighty difficult with inconsiderate people parking infront of their driveway!	9/26/2024 3:18 PM



LEGEND

--- SAN. or STM.	EXISTING SEWERS, SANITARY or STORM
--- MH, CB	EXISTING MANHOLE and CATCHBASIN
--- W	EXISTING WATERMAIN
--- G	EXISTING GASMAIN (ENBRIDGE)
--- T	EX. UNDERGND. TELECOMMUNICATIONS (BELL, EASTLINK)
--- H	EX. UNDERGND. HYDRO (WELLINGTON NORTH POWER)
--- F	EX. UNDERGND. FIBRE OPTIC (NIGHTMAN)
--- U	EX. UTILITY POLES
--- S	PROPOSED SANITARY SEWER
--- W	PROPOSED WATERMAIN
--- G	PROPOSED STORM SEWER
--- X	GRUBBING (TREE REMOVAL)
--- Y	REMOVE EXISTING CONC. SIDEWALK AND DRIVES
--- Z	REMOVE EXISTING ASPHALT



NOTE
 The locations of existing underground utilities are shown in an approximate way only and have not been independently verified by the owner or its representative. The contractor shall determine the exact location of all existing utilities before commencing work and agrees to be fully responsible for any damages which might be occasioned by the contractor's failure to exactly locate and preserve any and all underground utilities.

BENCHMARK INFORMATION
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BENCHMARK INFORMATION
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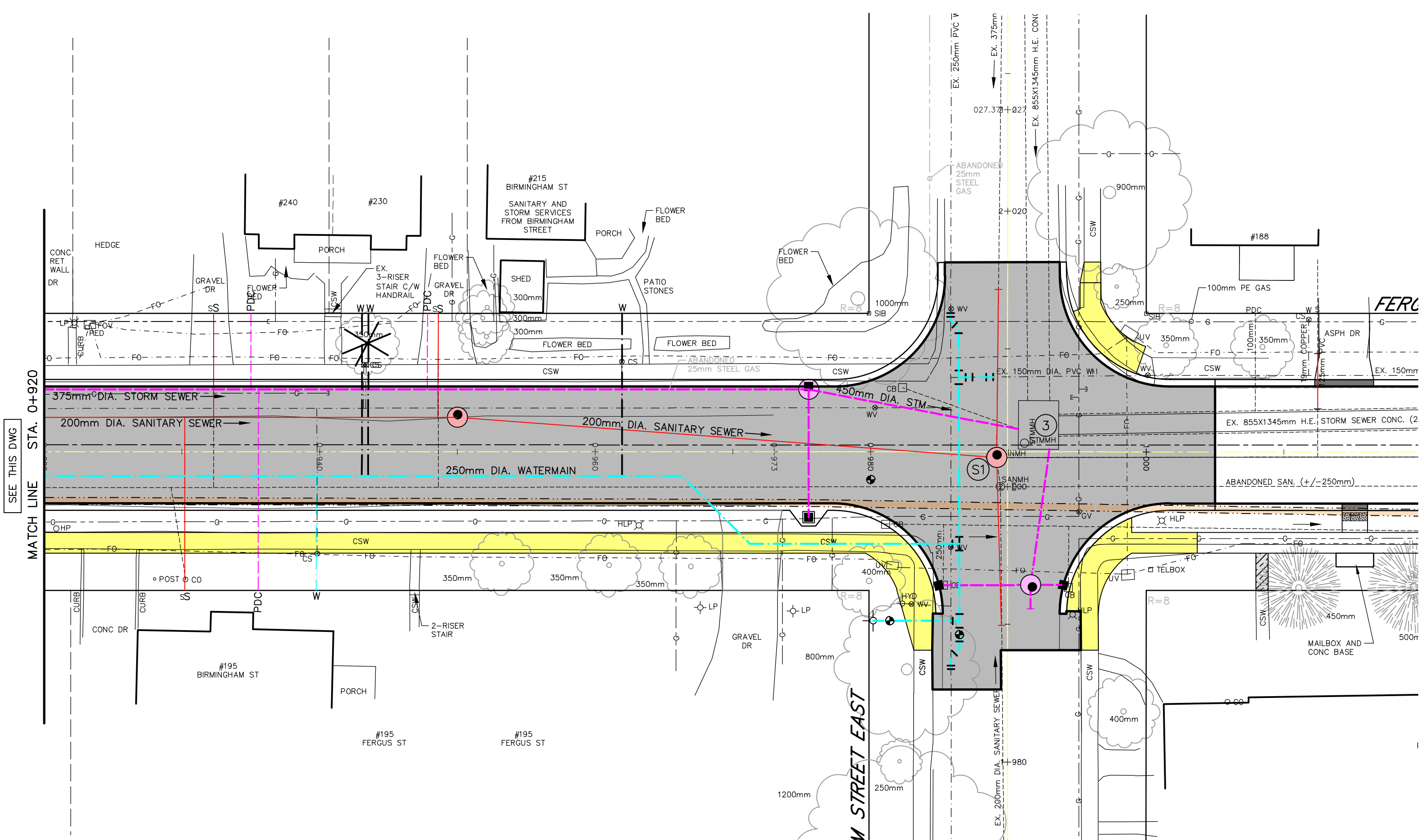
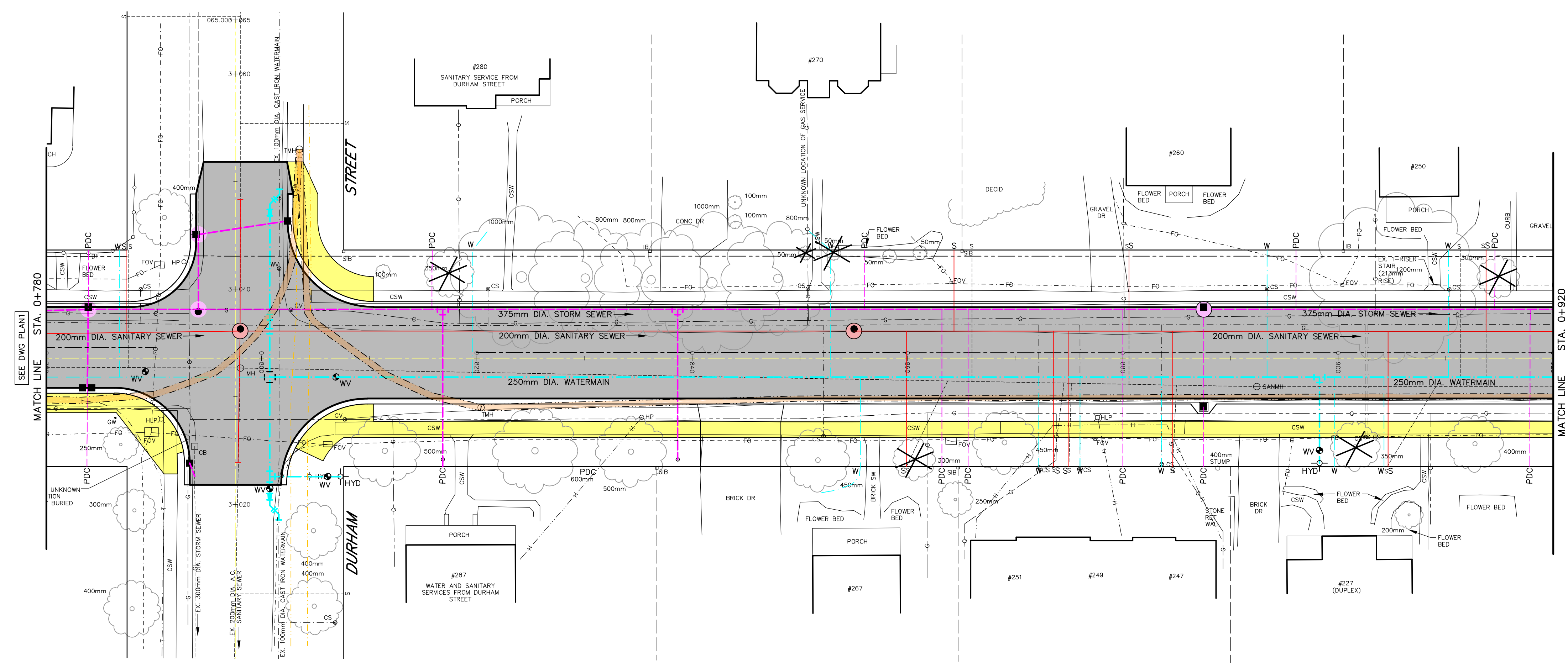
Design By: J.A.V. Checked By: I.S.D.

No.	DATE	REVISION
1	Sep. 16, 2024	Issued to Client for Review



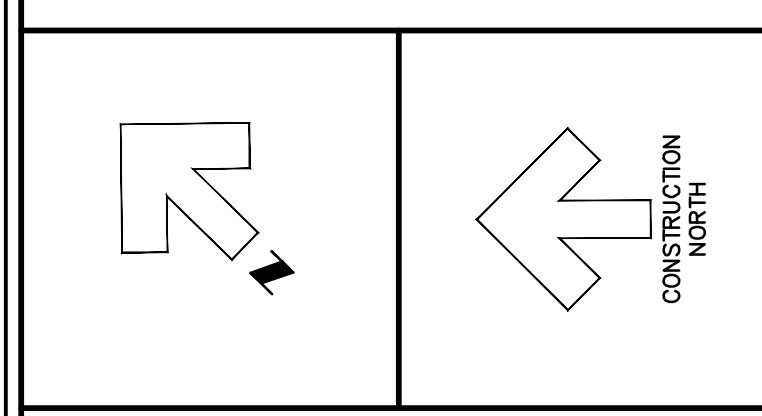
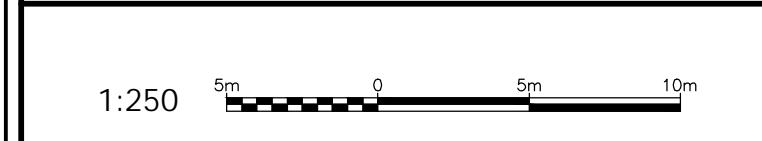
TOWNSHIP OF WELLINGTON NORTH
FERGUS STREET RECONSTRUCTION
 Plan View from Sligo Road to Sta. 0+780

Contract No. ?	Project No. 21340A,B
Scale (24x36) Horizontal : 1:250 Vertical : 1:50	Drawing No. Plan1



LEGEND

— SAN. or STM.	EXISTING SEWERS, SANITARY or STORM
— MH, CB	EXISTING MANHOLE and CATCHBASIN
— W	EXISTING WATERMAIN
— G	EXISTING GASMAIN (ENBRIDGE)
— T	EX. UNDERGROUND TELECOMMUNICATIONS (BELL, EASTLINK)
— H	EX. UNDERGROUND HYDRO (WELLINGTON NORTH POWER)
— FO	EX. UNDERGROUND FIBRE OPTIC (NIGHTMAN)
— U, B, T, H-B	EX. UTILITY POLES
— (Red dashed)	PROPOSED SANITARY SEWER
— (Blue dashed)	PROPOSED STORM SEWER
— (Green dashed)	PROPOSED WATERMAIN
— (Black dashed)	GRUBBING (TREE REMOVAL)
— (Yellow hatched)	PLACE CONC. SIDEWALK AND DRIVES
— (Grey hatched)	REMOVE EXISTING CONC. SIDEWALK AND DRIVES
— (Dark grey hatched)	PLACE HOT MIX ASPHALT (DRIVES 50mm HL-3 HOT MIX MISC.)
— (Light grey hatched)	REMOVE EXISTING ASPHALT
— (Red hatched)	REINSTALL SALVAGED BRICK PAVING UNITS
— (Black hatched)	REMOVE EXISTING ASPHALT
— (Black hatched)	DROP CURB (NOT INCLUDING TAPER)



NOTE
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BENCHMARK INFORMATION
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BENCHMARK INFORMATION
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Design By: J.A.V. Checked By: I.S.D.

No.	DATE	REVISION
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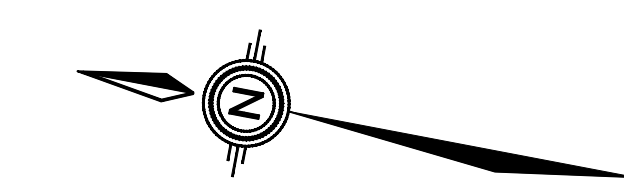
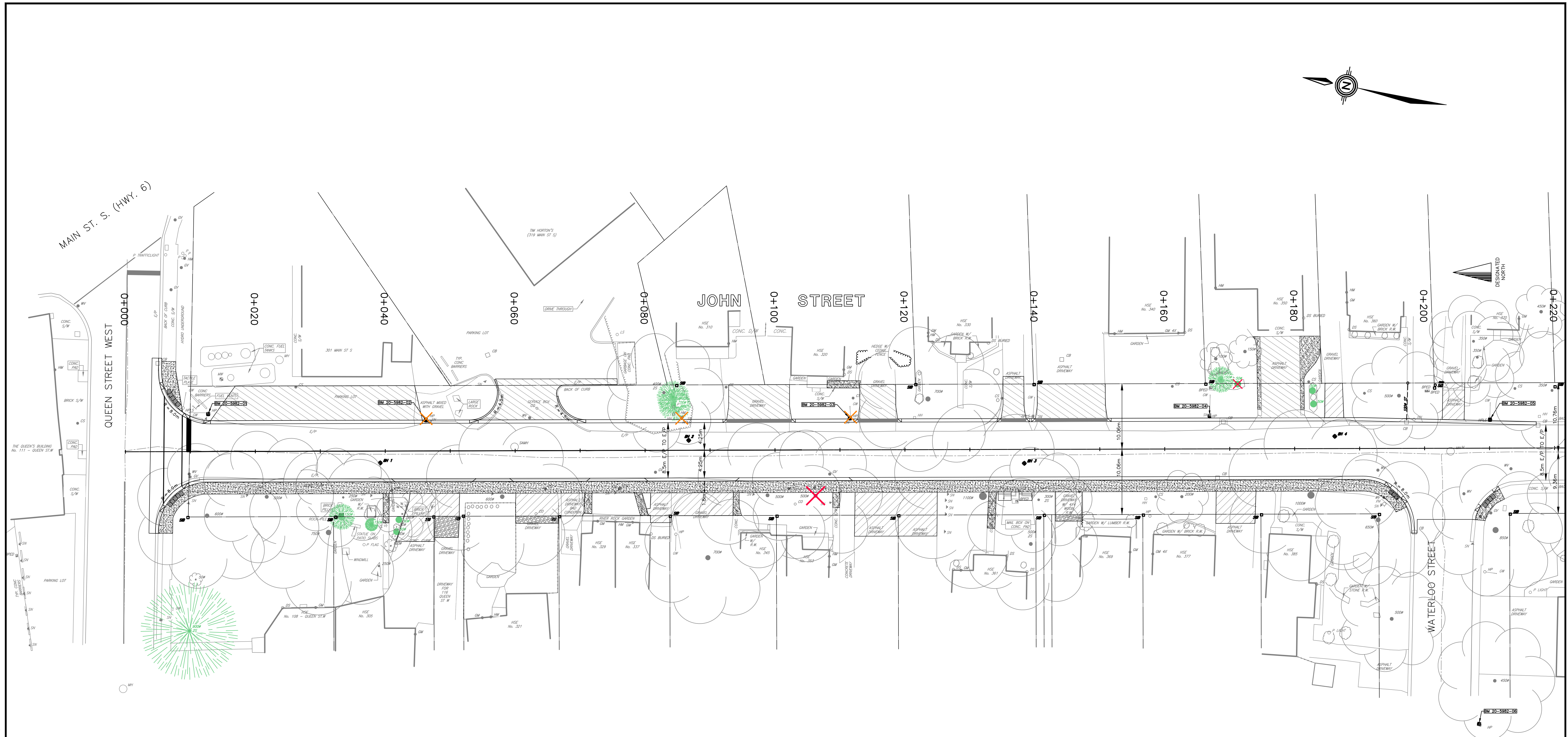


Goderich Mount Forest Sarnia

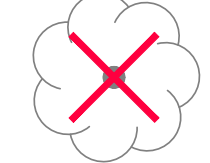



TOWNSHIP OF WELLINGTON NORTH
FERGUS STREET RECONSTRUCTION
 Plan View from Sligo Road to Sta. 0+780

Contract No. ?	Project No. 21340A,B
Scale (24x36) Horizontal : 1:250 Vertical : 1:50	Drawing No. Plan2



LEGEND:

-  TREE REMOVAL
-  HYDRO POLE REQUIRES RELOCATION DUE TO ENCROACHMENT ON CURB

DISCLAIMERS:
 1. ALL EXISTING ELEVATIONS & DIMENSIONS TO BE CONFIRMED ON SITE. THE LOCATION OF UTILITIES IS APPROXIMATE ONLY AND SHOULD BE DETERMINED BY CONSULTING THE MUNICIPAL AUTHORITIES AND UTILITY COMPANIES CONCERNED. THE CONTRACTOR SHALL PROVE THE LOCATION OF UTILITIES AND SHALL BE RESPONSIBLE FOR ADEQUATE PROTECTION AGAINST DAMAGE.

No	DATE	REVISION	INITIAL
0	JUN. 2024	PRELIMINARY	PFZ

**RECONSTRUCTION OF
JOHN STREET**

TOWNSHIP OF WELLINGTON NORTH
(MOUNT FOREST)

TOWNSHIP OF WELLINGTON NORTH
7490 SIDEROAD 7 WEST
KENILWORTH, ON N0G 2G0

**OPTION 1A
STANDARD LOCAL STREET**

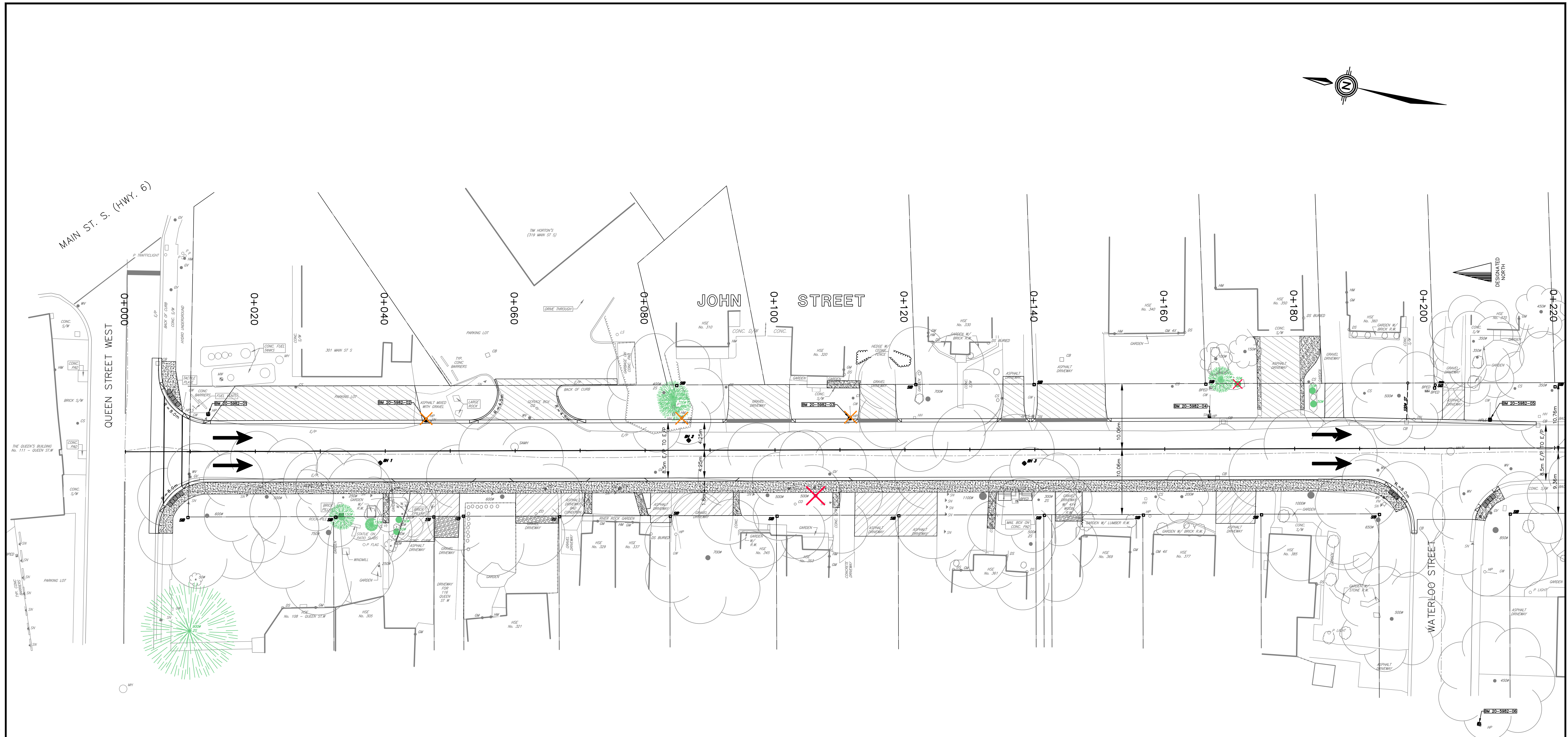
PROJECT No
M5982
DESIGNED BY: ASB
CHECKED BY: LS
APPROVED BY: PFZ
DATE: JUNE 2024



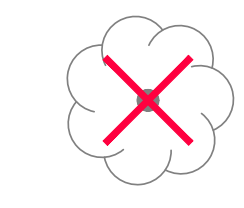
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(06/24) (07/17)
UNLESS OTHERWISE SHOWN

DRAWING NUMBER **SK-1**

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LEGEND:



TREE REMOVAL



HYDRO POLE REQUIRES RELOCATION DUE TO ENCROACHMENT ON CURB

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No	DATE	REVISION	INITIAL
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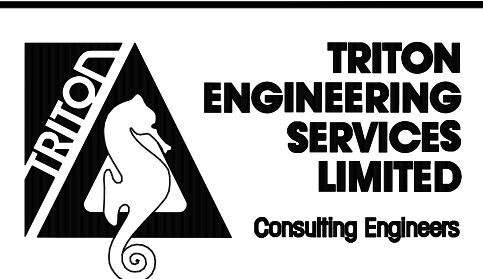
**RECONSTRUCTION OF
JOHN STREET**

TOWNSHIP OF WELLINGTON NORTH
(MOUNT FOREST)

TOWNSHIP OF WELLINGTON NORTH
7490 SIDEROAD 7 WEST
KENILWORTH, ON N0G 2G0

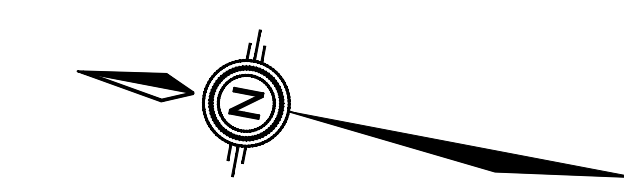
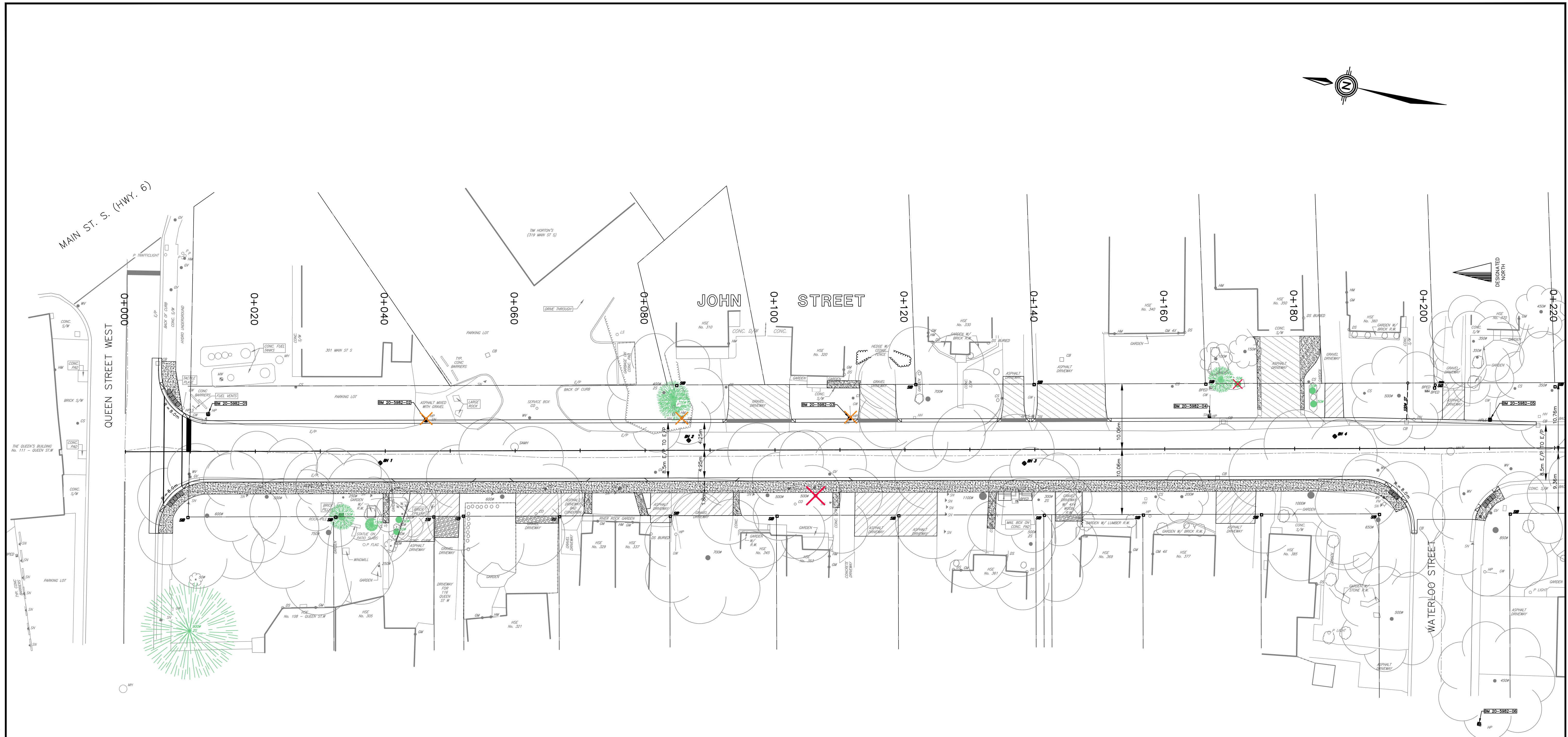
OPTION 1B
STANDARD LOCAL STREET - ONE WAY

PROJECT No
M5982
DESIGNED BY: ASB
CHECKED BY: LS
APPROVED BY: PFZ
DATE: JUNE 2024

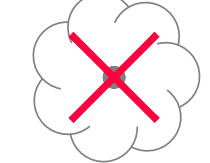



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(06/24) (07/17)
UNLESS OTHERWISE SHOWN

DRAWING NUMBER **SK-2**



LEGEND:

-  TREE REMOVAL
-  HYDRO POLE REQUIRES RELOCATION DUE TO ENCROACHMENT ON CURB

DISCLAIMERS:
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No	DATE	REVISION	INITIAL
0	JUN. 2024	PRELIMINARY	PFZ

**RECONSTRUCTION OF
JOHN STREET**
 TOWNSHIP OF WELLINGTON NORTH
 (MOUNT FOREST)

TOWNSHIP OF WELLINGTON NORTH
 7490 SIDEROAD 7 WEST
 KENILWORTH, ON N0G 2G0
OPTION 2
LOCAL URBAN STREET CROSS SECTION

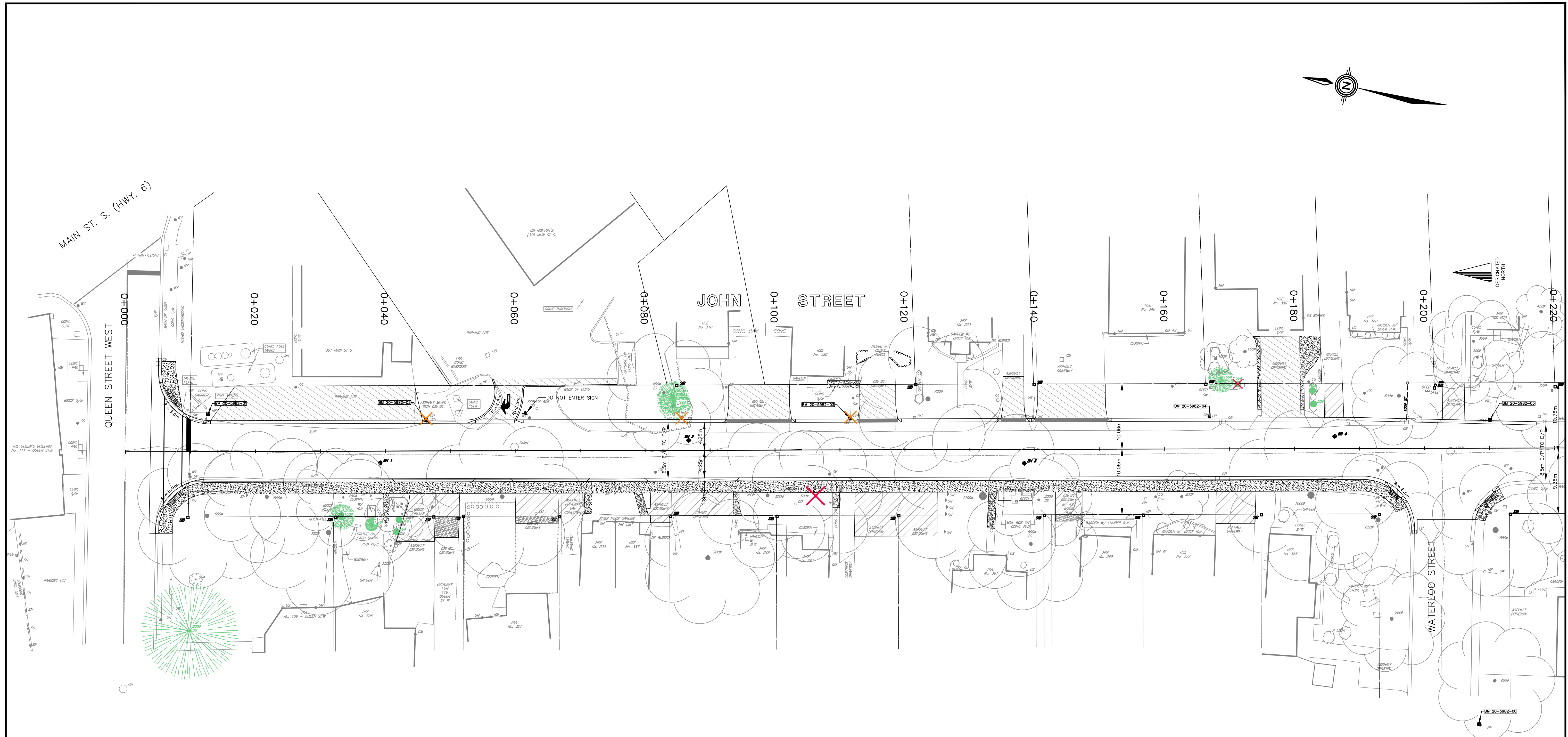
PROJECT No
M5982
 DESIGNED BY: ASB
 CHECKED BY: LS
 APPROVED BY: PFZ
 DATE: JUNE 2024



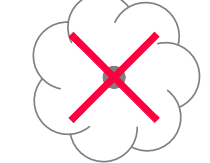

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 (06/24) (07/17)
 UNLESS OTHERWISE SHOWN

DRAWING NUMBER **SK-3**

C:\Wellington_north - map\01\m5982 - map\01\m5982 - john st recon (urban st w to waterloo st) mtl_fresh\jrd and stree\m5982_option_2.dwg - 2024-07-02 - addendum
 ARCH full bleed D (36.00 x 24.00 inches)



LEGEND:

-  TREE REMOVAL
-  HYDRO POLE REQUIRES RELOCATION DUE TO ENCROACHMENT ON CURB

DISCLAIMERS:
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No	DATE	REVISION	INITIAL
0	JUN. 2024	PRELIMINARY	PFZ

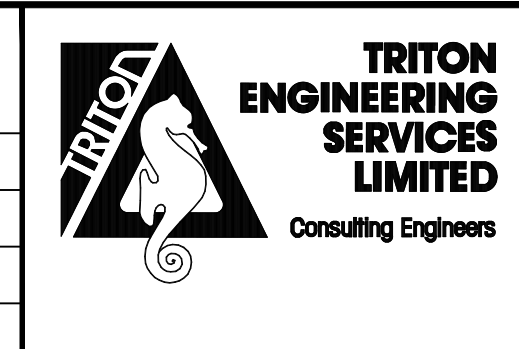
**RECONSTRUCTION OF
JOHN STREET**

TOWNSHIP OF WELLINGTON NORTH
(MOUNT FOREST)

TOWNSHIP OF WELLINGTON NORTH
7490 SIDEROAD 7 WEST
KENILWORTH, ON N0G 2G0

**OPTION 3 - LOCAL URBAN STREET
CROSS SECTION, RIGHT TURN EXIT
ONLY FROM 319 MAIN ST. S.**

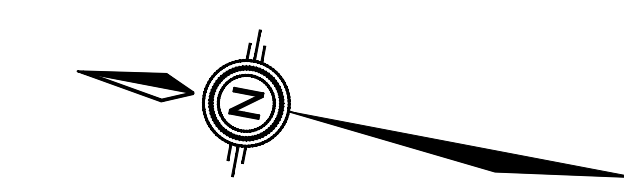
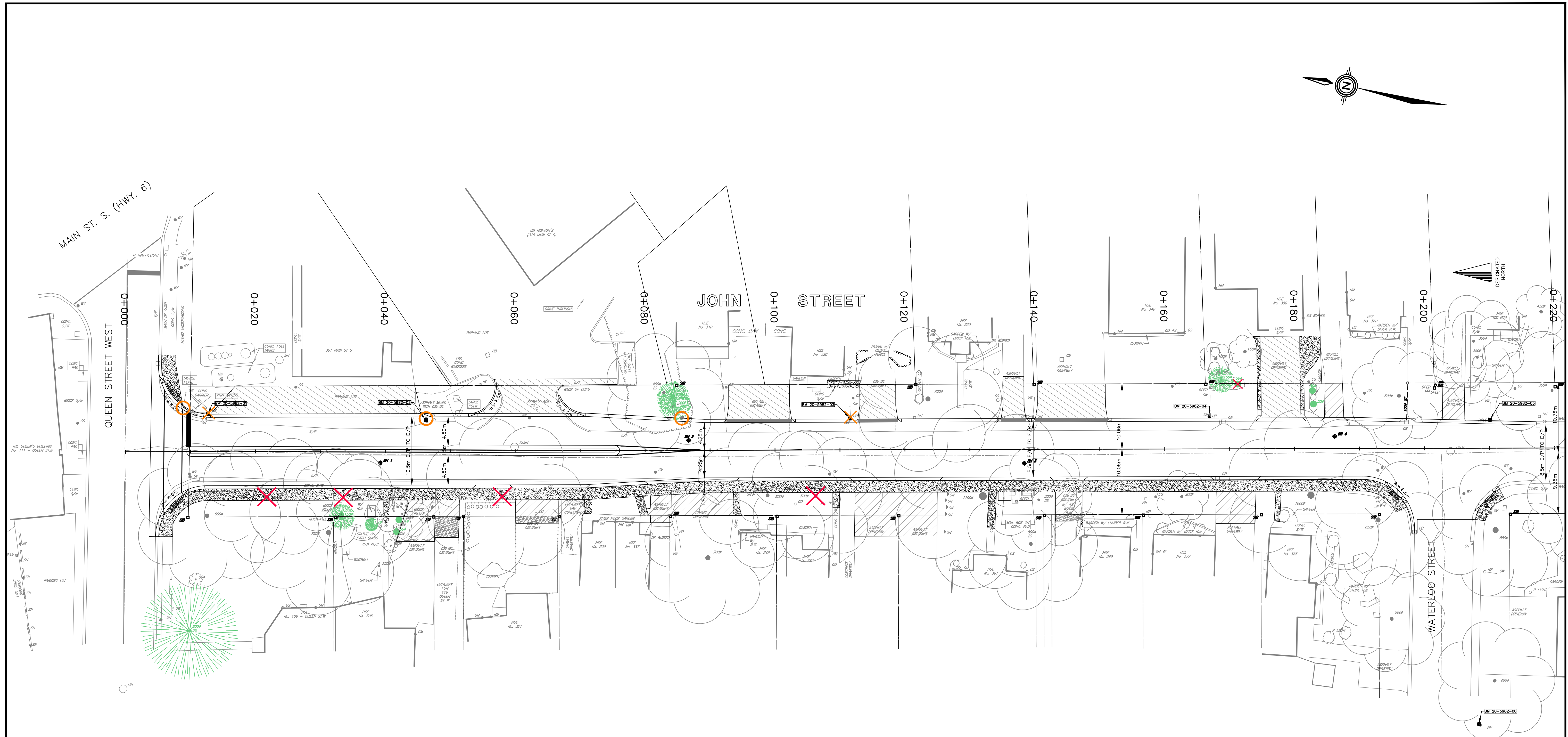
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DESIGNED BY: ASB
CHECKED BY: LS
APPROVED BY: PFZ
DATE: JUNE 2024



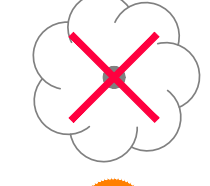


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DRAWING NUMBER **SK-4**

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LEGEND:

-  TREE REMOVAL
-  HYDRO POLE RELOCATION DUE TO CONFLICT
-  HYDRO POLE REQUIRES RELOCATION DUE TO ENCROACHMENT ON CURB

DISCLAIMERS:
 1. ALL EXISTING ELEVATIONS & DIMENSIONS TO BE CONFIRMED ON SITE. THE LOCATION OF UTILITIES IS APPROXIMATE ONLY AND SHOULD BE DETERMINED BY CONSULTING THE MUNICIPAL AUTHORITIES AND UTILITY COMPANIES CONCERNED. THE CONTRACTOR SHALL PROVE THE LOCATION OF UTILITIES AND SHALL BE RESPONSIBLE FOR ADEQUATE PROTECTION AGAINST DAMAGE.

No	DATE	REVISION	INITIAL
0	JUN. 2024	PRELIMINARY	PFZ

**RECONSTRUCTION OF
JOHN STREET**
 TOWNSHIP OF WELLINGTON NORTH
 (MOUNT FOREST)

TOWNSHIP OF WELLINGTON NORTH
 7490 SIDEROAD 7 WEST
 KENILWORTH, ON N0G 2G0
**OPTION 4
CENTRE MEDIAN**

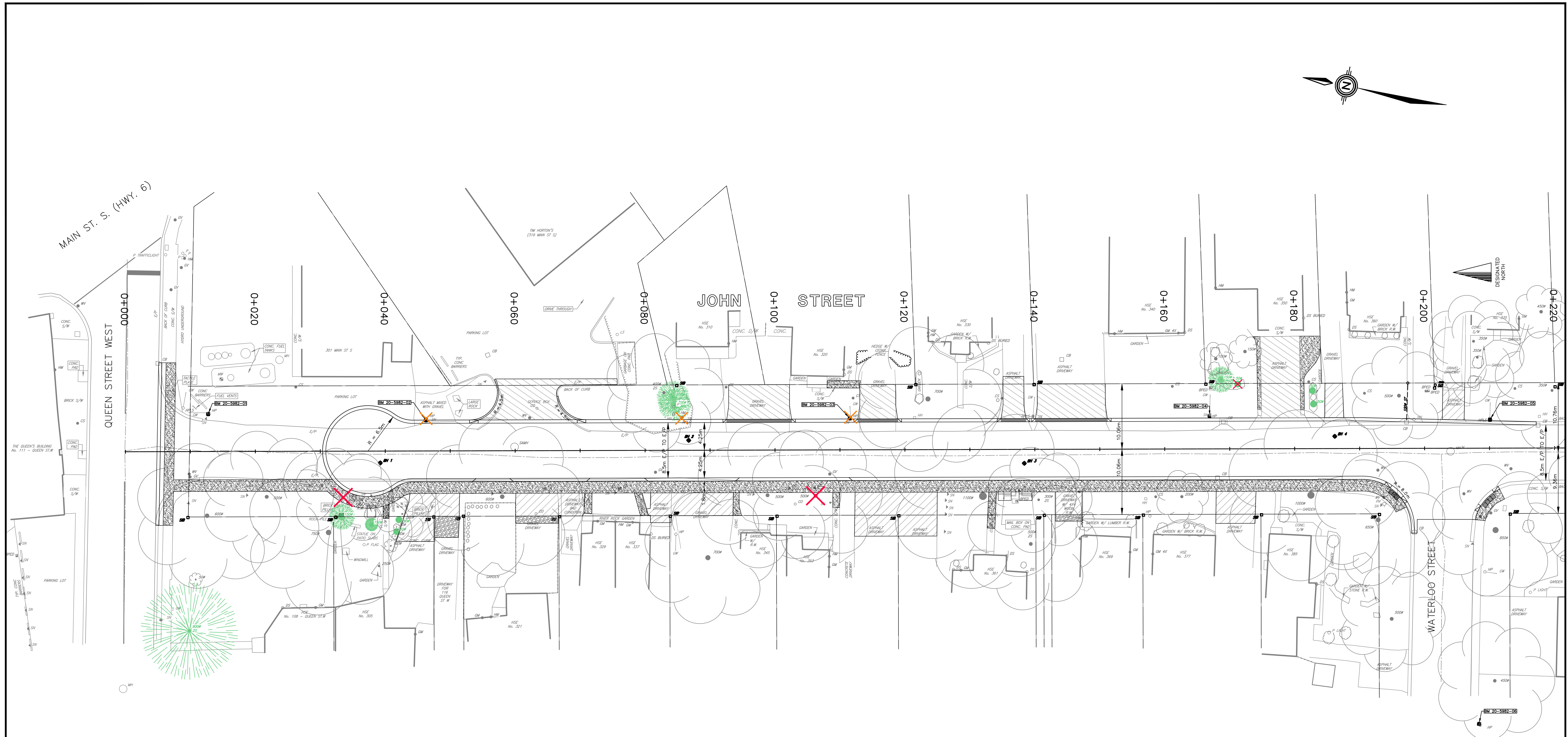
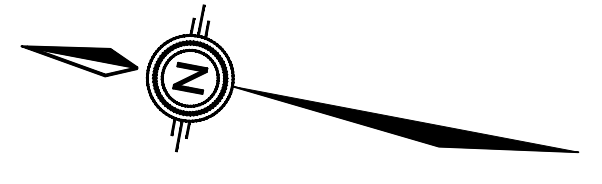
PROJECT No
M5982
 DESIGNED BY: ASB
 CHECKED BY: LS
 APPROVED BY: PFZ
 DATE: JUNE 2024



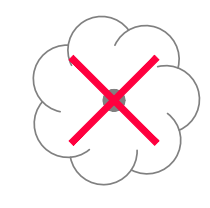
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 (06/24) (07/17)
 UNLESS OTHERWISE SHOWN

DRAWING NUMBER **SK-5**

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LEGEND:



TREE REMOVAL



HYDRO POLE REQUIRES RELOCATION DUE TO ENCROACHMENT ON CURB

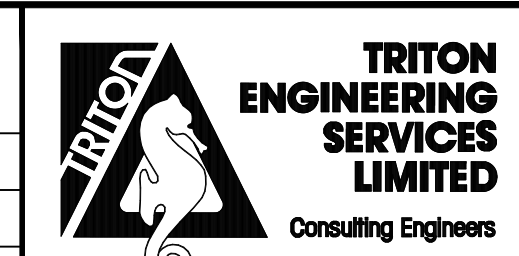
DISCLAIMERS:
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No	DATE	REVISION	INITIAL
0	JUN. 2024	PRELIMINARY	PFZ

**RECONSTRUCTION OF
 JOHN STREET**
 TOWNSHIP OF WELLINGTON NORTH
 (MOUNT FOREST)

TOWNSHIP OF WELLINGTON NORTH
 7490 SIDEROAD 7 WEST
 KENILWORTH, ON N0G 2G0
OPTION 5
 JOHN ST. CLOSED AT QUEEN ST.

PROJECT No	M5982
DESIGNED BY:	ASB
CHECKED BY:	LS
APPROVED BY:	PFZ
DATE:	JUNE 2024



SCALE:
 H: 1:300 H: 1:600
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 (06/24) (07/17)
 UNLESS OTHERWISE SHOWN

DRAWING NUMBER
SK-6